

P.H.V.

FILE NO. 90-79-7

RECEIVED
BOARD OF SUPERVISORS
SAN FRANCISCO
1979 FEB 14 AM 11:31

ORDINANCE NO. 124-79

1 BY _____
2 DESIGNATING THE MARKET STREET RAILWAY SUBSTATION AS A LANDMARK PURSUANT TO ARTICLE
3 10 OF THE CITY PLANNING CODE.

4 Be It Ordained by the People of the City and County of San Francisco:

5 Section 1. The Board of Supervisors hereby finds that the Market Street
6 Railway Substation located at 1190 Fillmore Street, being a portion of Lot 1 in
7 Assessor's Block 756, has a special character and special historical, architectural
8 and aesthetic interest and value, and that its designation as a Landmark will be in
9 furtherance of and in conformance with the purposes of Article 10 of the City
10 Planning Code and the standards set forth therein.

11 (a) Designation. Pursuant to Section 1004 of the City Planning Code, Chapter
12 II, Part II of the San Francisco Municipal Code, the Market Street Railway Substa-
13 tion is hereby designated as a Landmark, this designation having been duly approved
14 by Resolution No. 8040 of the City Planning Commission, which Resolution is on file
15 with the Clerk of the Board of Supervisors under File No. 90-79-7.

16 (b) Required Data. The descriptions of the location and boundaries of the
17 landmark site; of the characteristics of the landmark which justify its designation;
18 and of the particular features that should be preserved; as included in the said
19 Resolution, are hereby incorporated herein and made a part hereof as though fully
20 set forth.

21
22 APPROVED AS TO FORM:
23 GEORGE AGNOST
24 CITY ATTORNEY

RECOMMENDED
CITY PLANNING COMMISSION

25
26 By 
Deputy City Attorney

By 
Rai Y. Okamoto
Director of Planning

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29
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LM # 105

Passed for Second Reading
Board of Supervisors, San Francisco

..... MAR. 5 ... 1979 ...

Ayes: Supervisors Britt, Dolson, Gonzales, Horanzy, Hutch, ~~Kopp~~ Lau, Molinari, Pelosi, Renne, ~~Silver~~

~~Noes: Supervisors~~

Absent: Supervisors KOPP SILVER

..... *M. Brennan* Clerk

90-79-7

File No.

Approved

Read Second Time and Finally Passed
Board of Supervisors, San Francisco

..... MAR 12 1979

Ayes: Supervisors Britt, Dolson, ~~Gonzales~~, Horanzy, Hutch, ~~Kopp~~ Lau, Molinari, Pelosi, Renne, Silver.

~~Noes: Supervisors~~

Absent: Supervisors GONZALES KOPP

I hereby certify that the foregoing ordinance was finally passed by the Board of Supervisors of the City and County of San Francisco.

..... *M. Brennan* Clerk

..... *Deane Austin* Mayor

SAN FRANCISCO
CITY PLANNING COMMISSION
RESOLUTION NO. 8040

WHEREAS, A proposal to designate the Market Street Railway Substation as a Landmark pursuant to the provisions of Article 10 of the City Planning Code was initiated by the Landmarks Preservation Advisory Board on March 1, 1978, and said Advisory Board, after due consideration, has recommended approval of this proposal; and

WHEREAS, The City Planning Commission, after due notice given, held a public hearing on August 10, 1978 to consider the proposed designation and the report of said Advisory Board; and

WHEREAS, The Commission believes that the proposed Landmark has a special character and special historical, architectural and aesthetic interest and value; and that the proposed designation would be in furtherance of and in conformance with the purposes and standards of the said Article 10;

NOW THEREFORE BE IT RESOLVED, First, that the proposal to designate the Market Street Railway Substation as a Landmark pursuant to Article 10 of the City Planning Code is hereby APPROVED, the location and boundaries of the landmark site being as follows:

That portion of Lot 1 in Assessor's
Block 756 which occupies a frontage of
45'-2" on Fillmore Street and 201'-6"
on Turk Street

SECOND, That the special character and special historical, architectural, and aesthetic interest and value of the said Landmark justifying its designation are set forth in the Landmarks Preservation Advisory Board Resolution No. 174 as adopted on March 1, 1978, which resolution is incorporated herein and made a part hereof as though fully set forth;

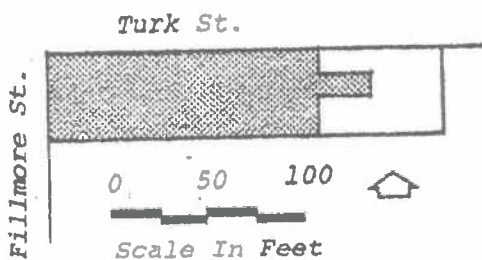
THIRD, That the said Landmark should be preserved generally in all of its particular exterior features as existing on the date hereof and as described and depicted in the photographs, case report and other material on file in the Department of City Planning Docket LM77-43; 78.2,

AND BE IT FURTHER RESOLVED, That the Commission hereby directs its Secretary to transmit the proposal for designation, with a copy of this Resolution, to the Board of Supervisors for appropriate action.

I hereby certify that the foregoing resolution was adopted by the City Planning Commission at its Regular meeting on August 10, 1978.

Lee Woods
Secretary

AYES: Commissioners Bierman, Dearman, Matoff, Mignola, Nakashima, Rosenblatt, Starbuck.
NOES: None.
ABSENT: None.
PASSED: August 10, 1978.



Owner: City and County of San Francisco (Municipal Railway)

Location: 1190 Fillmore Street
Southeast corner of Fillmore & Turk Streets

STATEMENT OF SIGNIFICANCE:

The Turk and Fillmore substation was one of the first and one of the largest of the substations built near the turn of the century when the street railroads of San Francisco were converted to electric power. It is now one of the last old time street railway substations still in regular use in western America (Smallwood, White Fronts Cars of San Francisco, 1971), but this function will soon cease. The structure, a handsome example of the brick and sandstone architecture of the time, stood through the earthquake and fire of 1906, and was essential in making the Fillmore Street line the first to be put back into service.

HISTORY:

In 1857 the Market Street Railroad Company was established, which built in San Francisco the first street railway on the Pacific Coast. The original form of power used was steam, but this was replaced by the horse in 1868. In 1882 the Railroad became the Market Street Cable Railway Company, whereupon much of it was converted to cable power. In 1891 the first electric railway in San Francisco (the San Francisco and San Mateo Railroad Company) was established, and a few months later the Metropolitan Railroad Company became the second electric railway. In 1893 most of the street railway lines in San Francisco were consolidated into the original Market Street Railway Company, which absorbed the Metropolitan Railroad a year later (1894), and began the conversion of many of its lines to electric power. In 1902 the various street railroads were again consolidated into the United Railroads of San Francisco, which continued the conversion to electric power.

As part of this conversion, plans were made by the original Market Street Railway for the Turk and Fillmore substation, and its construction may have begun as well. However, it appears that the United Railroads finished construction of the substation in 1902. This date is subject to some uncertainty since many records were lost in the fire of 1906, and existing information is unclear and sometimes contradictory. Charles Smallwood, in White Front Cars of San Francisco, 1971, states that it was built in 1896. This is not substantiated by other evidence, however. The substation does not appear on the 1899 Sanborn Maps of the City but it does appear on the Sanborn Maps updated to late 1905, indicating that it was built between those years. Other sources, while not precisely verifying the 1902 date, do, nevertheless, circumstantially suggest that year as the date of construction.

HISTORY:
(Continued)

No information is available regarding the architect of the substation.

The Turk and Fillmore substation converted 12,000 volts AC current received from the North Beach Power House to 550 volts DC, which was supplied to all the lines north of Market Street, and to the Geneva and Millbrae substations. The capacity of the substation was maximized in 1910 to 9,000 kilowatts, which was provided by six G.E. 10-pole, 1500 kilowatt, 300 rpm, 600 volt compound-wound rotary converters and eighteen air-blast transformers. One of the converter sets was removed in 1925. These sets, oddly, were not owned by the Market Street Railway, or by United Railroads but by PG&E and its predecessors.

The substation survived the 1906 earthquake and fire, and was instrumental in making the Fillmore Street line the first to be put back into service. The neighborhood then developed into a thriving business center.

In 1907 a small addition was made to the rear (east end) of the substation.

In 1921 United Railroads was taken over by the second Market Street Railway, the last of the great privately owned street railways, which existed until 1944, when the citizenry of San Francisco voted to merge it into the Municipal Railway. The Turk and Fillmore substation has been operated by Muni ever since, and is one of the last old time street railway substations still in regular use in western America (Smallwood 1971). It presently supplies power to Muni trolley coaches in the western part of the City, but this function will soon be transferred to a new substation now being constructed at Sutter and Fillmore Streets, and the Turk and Fillmore substation will be declared surplus.

ARCHITECTURE:

The substation is constructed of red brick and reinforced concrete, with sandstone trim. It measures 152.75 feet by 50 feet. There are two floors; the first floor measures 120.8 feet by 45 feet, with a ten foot ceiling. The main floor measures 140.8 feet by 45 feet, with a thirty foot ceiling at the apex. The windows are double-hung. The eleven main floor windows on the west (two) and north (nine) sides, which are slightly recessed, are a tall rectangular shape, with a semicircular sunburst pattern window and brick arch on top of each. The main entrance on the west end is similarly capped by a larger sunburst design window and brick arch. The eleven first floor windows, directly below the main floor windows, are a standard rectangular shape. Two horizontal projecting brick courses also run along these two sides. One separates the first and main floors; the other continues the line between the sunburst and rectangular sections of the main floor windows. There are three main floor windows on the east end. Two are rectangular with a flatter brick arch over each; the third (over the rear entrance) is also rectangular but is the only main floor window without an arch. There are no windows or other adornments on the south side, since it originally

ARCHITECTURE:
(Continued)

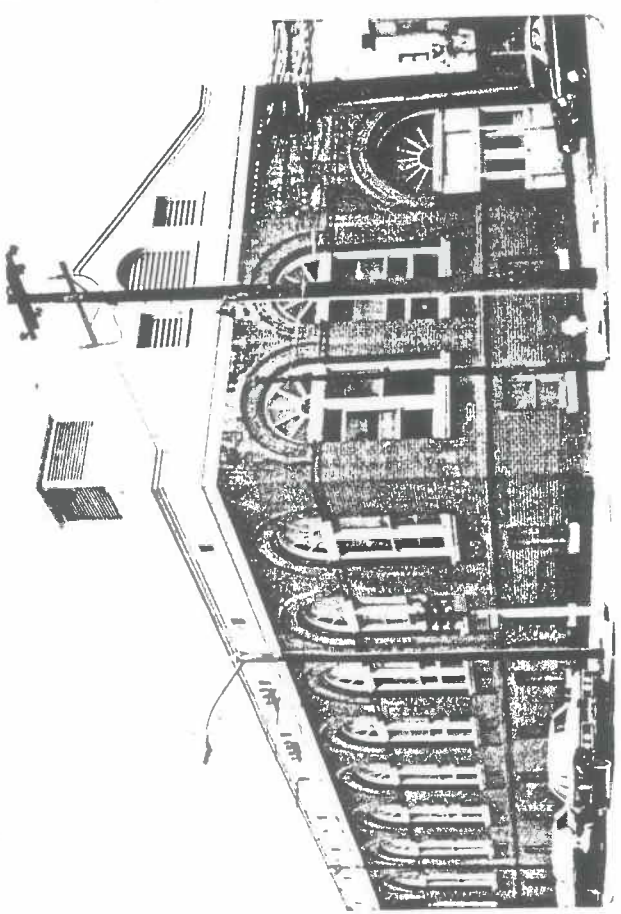
faced another building. This building was subsequently replaced by the existing park. The peaked roof is supported by steel trusses at about 40 degree angles, and contains a skylight running lengthwise along part of the peak. Near the peak on the walls at each end are several louvered ventilators.

SURROUNDING LAND
USE AND ZONING:

The property is zoned C-2 (Community Business District) and is in a 50-X Height and Bulk District. Adjacent land uses reflect the zoning and are a mixture of commercial and residential uses. A landscaped area for public use adjoins the building on the south.

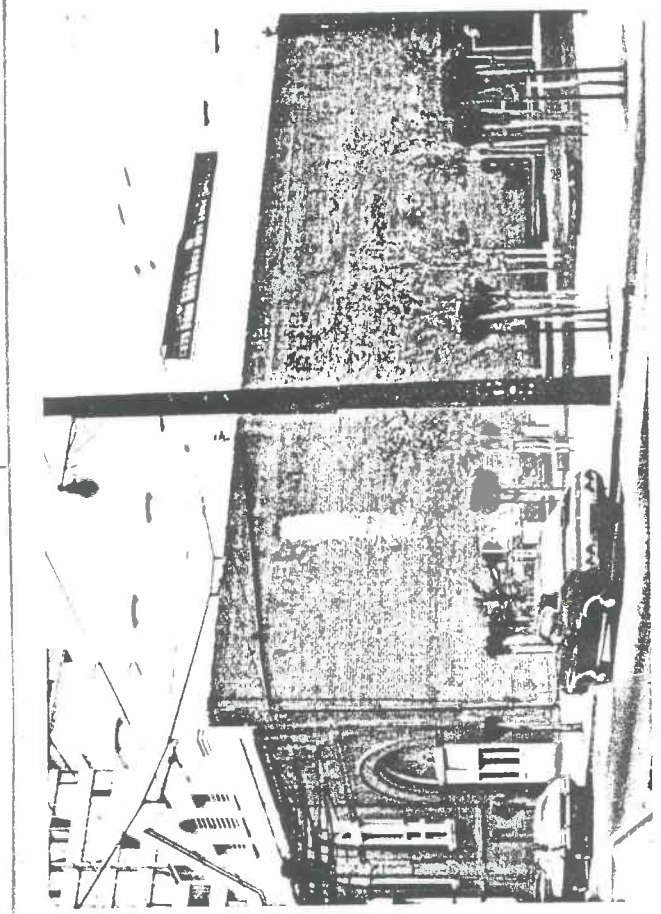
RESEARCH:

The research for and writing of the report was done by Garth Casaday.

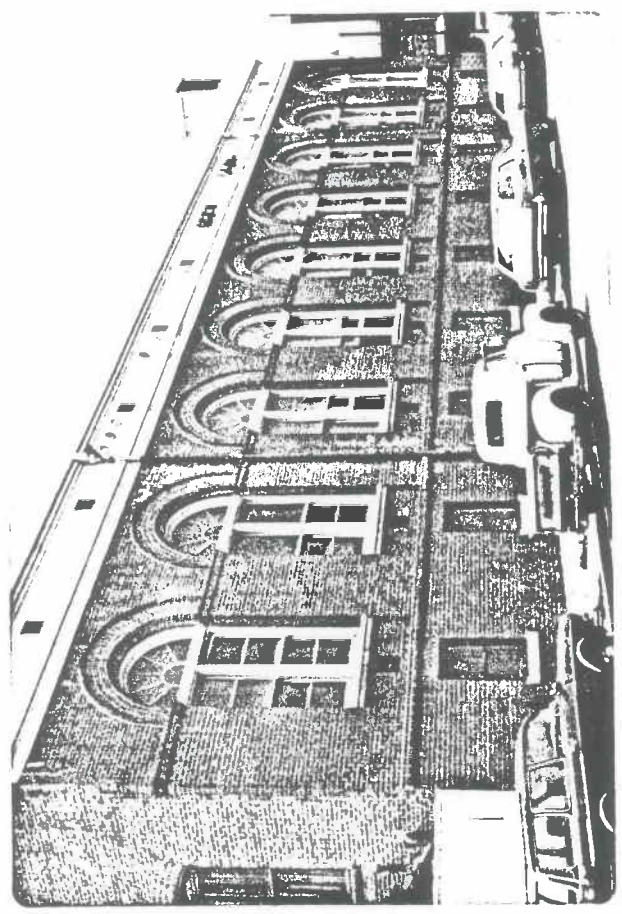


FILLMORE STREET (West) Elevation

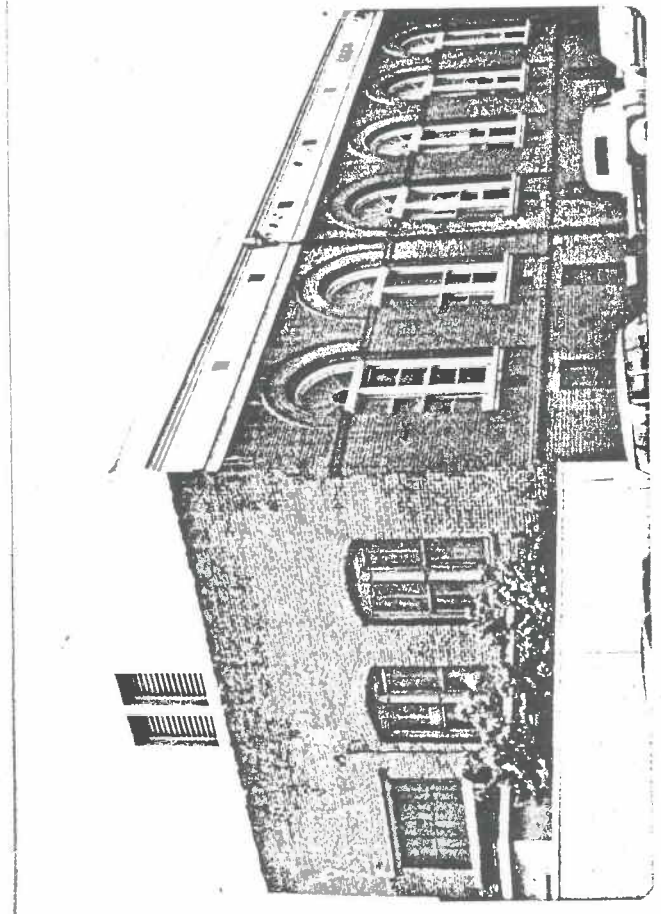
TURK STREET



SOUTH Elevation (Blank Wall) Note landscaping of adjacent area.



TURK STREET (North) Elevation



REAR (East) Elevation

PHOTOS TAKEN IN 1977

